

LAND OFF ASPERTON ROAD WIGTOFT, BOSTON

ARCHI-SCAPE



DESIGN JUSTIFICATION STATEMENT

November 2022



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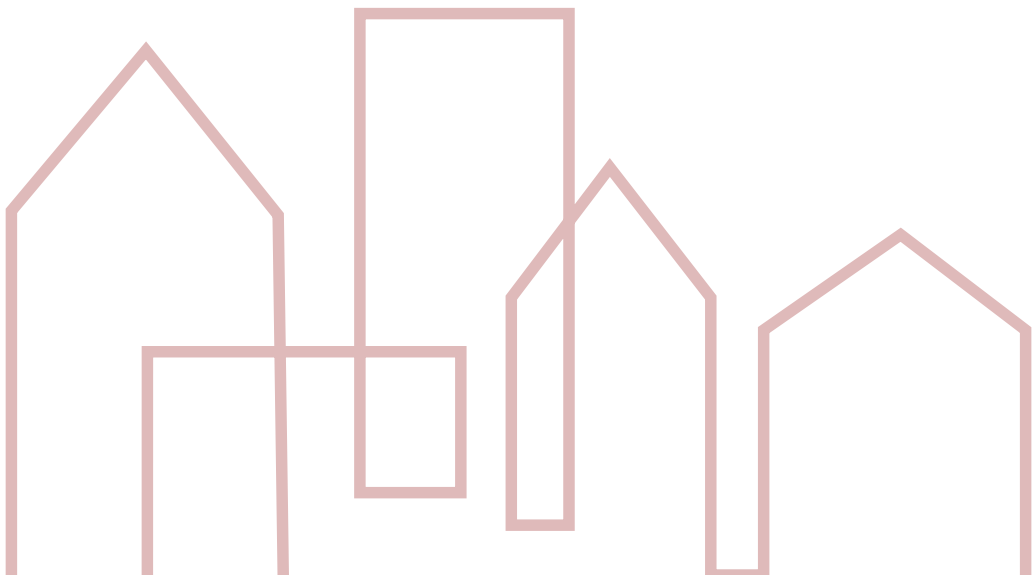
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1. BACKGROUND



1.1 SCOPE OF THIS DOCUMENT

1. Background

An overview of the site location, site photos, local heritage - Wigtoft Village Conservation Area, the approved Outline Planning Permission and relevant Planning Policy.

2. Description of Proposal

A detailed overview as to the design principles and specification underpinning the proposal as well as justification with regards to the Conservation Area and the Outline Planning Permission.

3. Architectural Appearance

A detailed overview of the architectural appearance of the proposed scheme as well as justification with regards to the Conservation Area and the Outline Planning Permission.

4. Planning Conditions

Planning condition discharge of relevant conditions attached to the Outline Planning Permission.

Scope of this Design Justification Statement

This Design Justification Statement (DJS) has been prepared by Archi-Scape Ltd on behalf of Mr S Collingwood, to accompany the Reserved Matters Application, for the construction of 9no. dwellings within a parcel of land off Asperton Road, Wigtoft, shown edged red on the Site Location Plan approved as part of the Outline Planning Application ref: B/19/0457/OUTL.

This DJS has also been written in accordance with industry recognised best practice guidance including the *CABE Design and Access Statements - How to Write, Read and Use Them*.

This document has been prepared to demonstrate how the detailed proposal has been designed to be broadly in accordance with the approved Outline Planning permission and Illustrative Layout submitted as part of the approval.

A number of additional reports and surveys have been produced to supplement the application and should therefore be read in conjunction with this DJS.

The Application

The application seeks approval of the reserved matters, being layout, scale, appearance, landscaping and access for the construction of 9no. residential dwellings together with associated landscaping, highway infrastructure and sustainable drainage system.

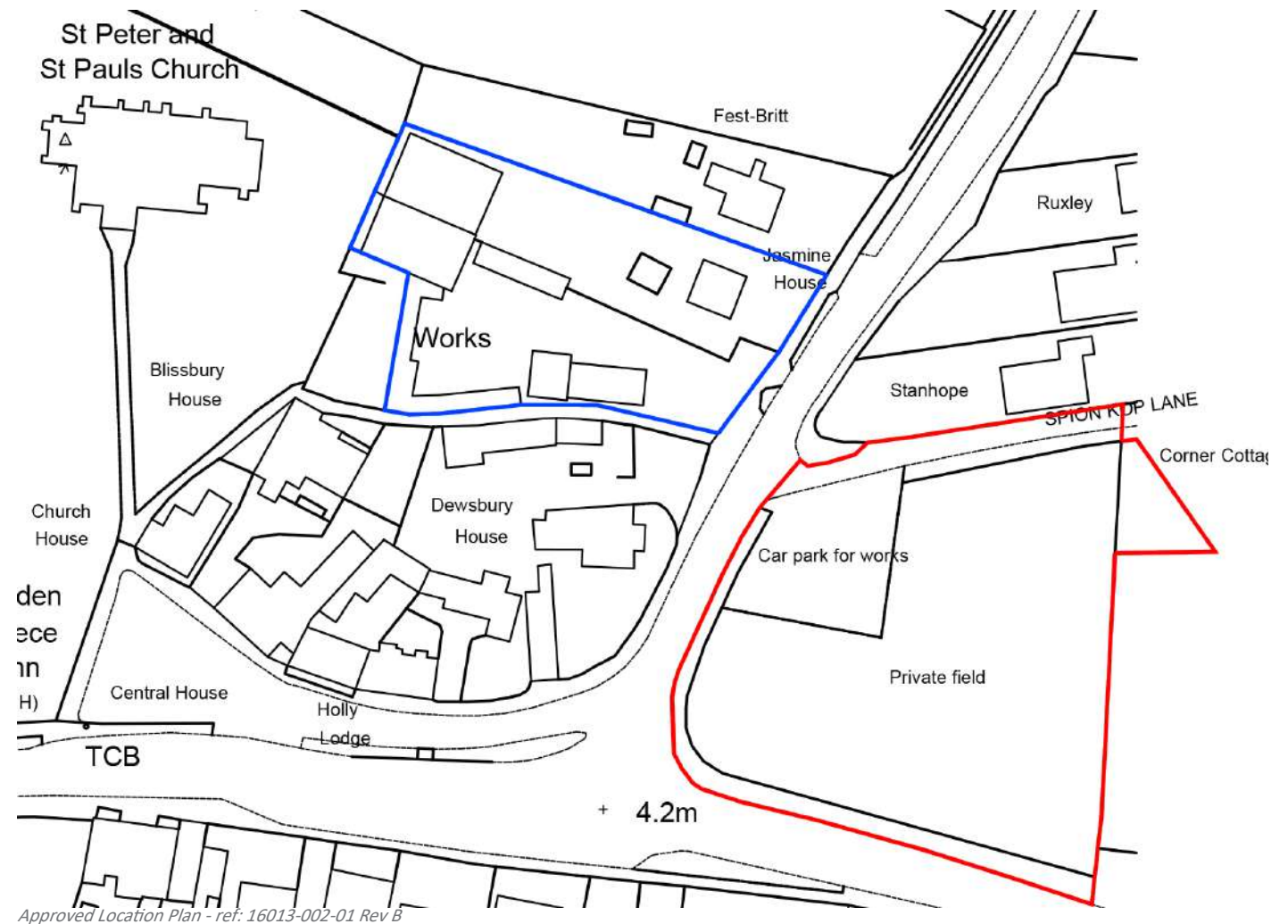
1.2 SITE LOCATION & DESCRIPTION

The site to which this application relates is located within the Parish of Wigtoft, approximately 5 miles south west of Boston. Most of the site is situated within the Wigtoft Village Conservation Area.

The site is bounded by three roads with Main Road to the south, Asperton Road to the west and Spion Kop Lane to north. Residential dwellings encompass the site to the north, west and southern peripheries. A vehicular repair shop (Scania) is situated immediately to the east and there is a small agricultural workshop to the north west.

The site itself is approximately 0.31 ha and comprises mainly of open grassland with a small hard surfaced parking area within the north west corner. The topography of the site is distinctly flat.

The principle for residential development has been established following the granting of the Outline Planning Application B/19/0457/OUTL in 2020.



1.3 SITE IMAGERY



1.4 HERITAGE - WIGTOFT VILLAGE CONSERVATION AREA

The southern part of the site falls within the Wigtoft Village Conservation Area. The Outline Planning Application (OPP) decision notice stipulates that the detailed proposal would require careful consideration as to the design of the dwellings to ensure the development does not have an urban appearance which could be detrimental to the Conservation Area.

This section demonstrates the process undertaken with regards to the contextual analysis and the prominent building elements and architectural details identified, which are referenced within the proposed house types in order to reinforce the local character and therefore not detract but benefit the Conservation Area as a result.

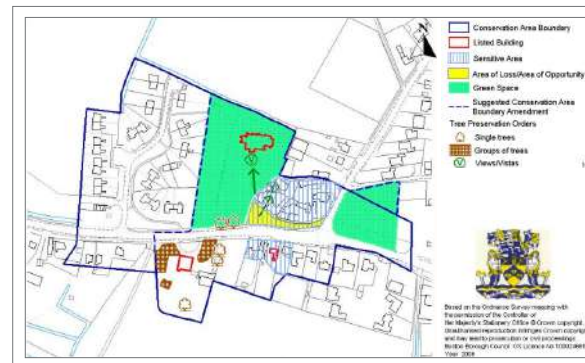
The Heritage Statement, submitted within the OPP, describes the conservation area as follows:

Wigtoft is typical of many settlements that grew up along main routes in and out of villages. It has a linear character with buildings strung out along its main spine road. There are distinctive historic buildings of two storeys, but the townscape is by no means homogenous.

The adjacent plan identifies the listed buildings within the vicinity, the closest of which is located approximately 80 metres to the southwest adjacent Main Road. There is a group of buildings to the north of Main Road, between the application site and the 'exceptional' St. Peter and St. Paul's Church, which make an important and characterful contribution to the Area through their arrangement and architectural appearance.



Wigtoft Conservation Area & associated Listed Buildings



Wigtoft Conservation Area



Grade I Listed - Church of St Peter & Paul's



Grade II Listed - The Vicarage, Main Road



Grade II Listed - Stanhope Cottage, Main Road

1.4 HERITAGE - WIGTOFT VILLAGE CONSERVATION AREA

The contextual analysis undertaken has included a comprehensive investigation of the entire village, with a particular focus on the Conservation Area 'core' and listed buildings, as defined within the Wigtoft Conservation Area Appraisal. Later additions to the Area have also been studied to understand how they have responded to the character of the Area.

The buildings in the Conservation Area are no higher than two storey. In the core area they are grouped quite tightly together and at the back of pavement or set slightly back. These buildings are all eaves onto the street. They are aligned to form a potentially interesting space at the heart of the village. Their details vary and the eaves lines and the use of gable chimney stacks gives interesting profiles to the roofs. As with most traditional buildings the roofs are narrow gutted.

The Area Appraisal highlights the following materials as being prominent within the Conservation Area:

The main walls are built in an orange red brickwork, the earlier bricks being handmade and the later Victorian ones are machine made, smoother and more orange. Quite a number of buildings in the village are either rendered or painted bricks. The colour used is generally white. Roofing materials are Welsh slate and clay pantile.

A fundamental objective of the proposal is to create a development which sits harmoniously within the Conservation Area. In order to achieve this and avoid an overly pastiche scheme, appropriate architectural details and materials have been pinpointed, as demonstrated to the right. These elements are considered to be consistently prominent within the Area and capable of being subtly incorporated into the elevations to reference local identity and stand the test of time.



Main Road - within 'Core' Grouping



Grade II Listed - Stanhope Cottage, Main



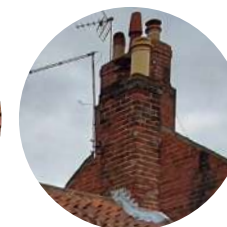
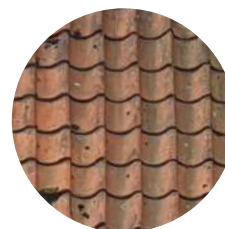
Main Road - within 'Core' Grouping



Asperton Road - within 'Core' Grouping



Main Road - modern building with traditional vernacular detailing



1.5 OUTLINE PLANNING PERMISSION

The site received Outline Planning Permission (OPP) for the residential development of up to 9no. dwellings with all matters reserved for later approval and subject to a number of planning conditions, a number of which are addressed later in the document.

The adjacent Illustrative Layout is not approved as part of the OPP but was produced in consultation with the Local Authority. The Decision Notice makes reference to the indicative design and layout not being acceptable, however it is acknowledged, by virtue of the OPP, the principle of development has been accepted.

Key principles of the OPP, which have been considered fundamental and continued within the proposal, are as follows:

- Quantum of development of up to 9no. units;
- Building frontages to Asperton Road and Main Road;
- Building set back behind existing landscaping (with enhancement) along southern and western edges;
- Appearance to compliment surrounding unique character;
- Dwellings to incorporate a range of details appropriate to the local vernacular;
- Points of access taken from Main Road, Asperton Road as well as Spion Kop Lane to serve the proposal;
- Buffer to eastern edge between the site and Scania;
- Existing trees and hedgerows to be retained and enhanced with additional native landscaping.



Illustrative Layout ref: 16013-002-01 Rev B

1.6 PLANNING POLICY BACKDROP

This section provides an overview of relevant planning policy relating specifically to design which have influenced the proposal.

South East Lincolnshire Local Plan

Policy 4: Design of New Development, requires all development to 'create distinctive places through the use of high quality and inclusive design and where possible make innovative use of local traditional styles and materials'. The following points have been considered within the proposal:

- 1. creating a sense of place by complementing and enhancing designated and non designated heritage assets; historic street patterns; respecting the density, scale, visual closure, landmarks, views, massing of neighbouring buildings and the surrounding area;
- 2. distinguishing between private and public space;
- 3. the landscape character of the location;
- 5. the provision of facilities for the storage of refuse/recycling bins, storage and/or parking of bicycles and layout of car parking;
- 8. crime prevention and community safety;
- 9. the orientation of buildings on the site to enable the best use of decentralised and renewable low-carbon energy technologies for the lifetime of the development;
- 11. residential amenity;
- 12. the mitigation of flood risk through flood-resistant and flood-resilient design and sustainable drainage systems;
- 14. the provision of appropriate landscaping and its use to enhance biodiversity and green infrastructure;
- 16. the appropriate treatment of façades to public places and the design of shop frontages and signage.

Policy 14: Providing a Mix of Housing, whilst this policy is relevant for developments of 10 or more dwellings, the proposal has sought to provide a range of housing including two, three and four bedroom homes.

Policy 27: Climate Change and Renewable and Low Carbon Energy;

A. Climate Change, all development proposals are required to demonstrate that consequences of current climate change have been addressed, minimised and mitigated by:

- 1. employing a high-quality design;
- 2. the adoption of the sequential approach and Exception Test to flood-risk and the incorporation of flood-mitigation measures in design and construction to reduce the effects of flooding;
- 5. incorporating measures which promote and enhance green infrastructure and provide an overall net gain in biodiversity;

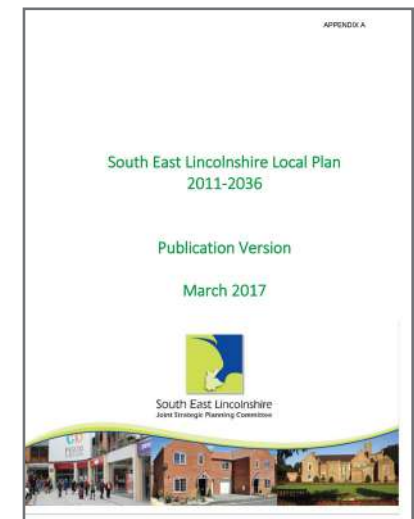
B. Renewable Energy, will be permitted provided, individually, or cumulatively, there would be no significant harm to:

- 1. visual amenity;
- 2. residential amenity in respect of: noise, fumes, odour, vibration, shadow flicker, sunlight reflection, broadcast interference, traffic;
- 6. heritage assets;
- 7. the natural environment.

Policy 31: Vehicle and Cycle Parking, all new development should provide vehicle and cycle parking, in accordance with the minimum Parking Standards adopted by the Local Authority. Parking for residents and visitors should be integral to the design and form of all new development, and should ensure that parking spaces are fit for their intended use in terms of size and design.

Appendix 4: Car Parking Standards:

- Within the curtilage:
- 2 spaces for dwellings with up to 3 bedrooms
- 3 spaces for dwellings with 4 or more bedrooms
- A garage can count as one space if it is 2.6m x 5.6m internal width, with an additional 1m at the end to park cycles.



2. DESCRIPTION OF PROPOSAL



2.1 PROPOSED LAYOUT

The proposed Site Layout has been designed in response to the detailed analysis undertaken and illustrated within the preceding chapters. Key drivers and design principles are as follows:





1. 9no. dwellings of mixed typology and size, each with a dedicated private rear garden;
2. Buildings orientated to create a perimeter block form and thus provide an attractive and active frontage to Main Road, Asperton Road and Spion Kop Lane;
3. Build line set back behind landscaped strip (along southern, western and northern edges) comprising existing vegetation and new native landscaping;
4. Existing hedge along boundary to Scania retained and enhanced to maintain and improve the visual screening;
5. Opportunities to create improved habitats for a biodiversity net gain within the landscaped areas;
6. Points of access taken from Main Road, Asperton Road as well as Spion Kop Lane to serve the proposal;
7. New footpath incorporated along Main Road and Asperton Road.



Detailed Site Layout

2.2 USE & AMOUNT

The proposal comprises 9no. residential dwellings (Class C3) in accordance with the OPP. A range of dwelling typologies and sizes are proposed, as recommended by Policy 1 of the Local Plan.

LEGEND	
House Type 1 - 2 Bedrooms	
House Type 2 - 3 Bedrooms	
House Type 3 - 4 Bedrooms	
House Type 4 - 4 Bedrooms	



House Type Plan

2.3 SCALE & MASSING

The contextual analysis undertaken has highlighted that two storey dwellings with front to back roofs dominate the conservation area. It was also noted the OPP Decision Notice made reference to the requirement of providing 'differing roofscapes'. Therefore, it has been considered most appropriate for the proposal to comply with this.

The house types proposed are all two storey with a roofscape reflective of the character of the traditional buildings within the Conservation Area, particularly those within the core grouping. Variations to the roof line profiles have been introduced in the form of chimneys, gable parapets and the front gables of house type 3 (plot 6) which also help to articulate the corner.



2.4 HARD & SOFT LANDSCAPING

Hard Landscape

Hard landscape materials have been specified to complement the Conservation Area and add a sense of quality to the proposal. Changes in material offer a subtle but clear transition from the public realm to private areas. Permeable solutions are proposed to assist with the surface water strategy (subject to detail design) in line with recommendations within the approved Flood Risk Assessment.

Indicative Soft Landscape Strategy

The indicative strategic placement of planting has been designed to enhance the visual amenity of the streetscene and soften hard elements of the proposal. A planting schedule will make allowance, in due course, to ensure the proposal could flourish all year round.

The layout has been designed to retain the vast majority of the existing trees. A robust tree protection plan would be implemented throughout construction to ensure no accidental damage could occur while construction activities take place.



LEGEND

Hard Landscaping

	Denotes Private Drive Permeable Block Paving Herringbone Bond
	Denotes Plot Parking Permeable Tegula Block Paving Bond
	Denotes Plot Paths Flagstones
	Denotes Bitmac Footpath
	Denotes Replacement Bitmac To Spion Kop Lane
	Denotes Gravel Margin

Soft Landscaping

	Denotes Existing Retained Vegetation
	Denotes Proposed Tree Planting
	Denotes Proposed Shrub Planting
	Denotes Proposed Hedge Planting
	Denotes Grassed Areas

2.5 BOUNDARY TREATMENTS

The boundary treatments throughout the Conservation Area vary considerably meaning there is no real distinction or uniformity which could be referenced. Brick, stone, metal railings, box hedges as well as brick piers with hedging are all present, some dwellings within the village core area have no physical front boundary between the public realm and private curtilage.

Therefore, the specified boundary treatments vary through the proposal so as to continue to enhance the development and the Area in an understated way. The southern and western peripheries of the development will be clearly defined by a new soft hedge boundary. Prominent plot boundaries, within the proposal, are proposed as screen walls. Closeboard fencing will separate the intermediate boundaries separating the rear amenity spaces and ensuring privacy for the future occupants.



1. Hedge planting



2. Screen Wall

- LEGEND
- 1.8m Closeboard Fence

1.8m Screen-wall

1.8m Timber Gate

Hedge Planting

Existing Retained Hedgerow
-



Boundary Treatments Plan

2.6 NEIGHBOUR AMENITY

Policy 4: Design of New Development, of the Local Plan requires new development to consider residential amenity. Consideration as to existing neighbouring properties have influenced the layout, not least in the orientation of the buildings to provide a positive outlook, but also in terms of separation distances.

No substantial harm is caused to the residential amenity in terms of overlooking, overshadowing or overbearing. The existing buildings along both Main Road and Asperton road have a front to front relationship with a separation distance of over 28m (nearest). The front to side relationship of plot 9 and Stanhope has been bought back (from the OPP) to 9.7m with a landscape screening strategically positioned in between to further maintain privacy.



2.7 ACCESS

The OPP Decision Notice makes reference to a number of comments from the Highway Consultee, which the detailed layout has taken into account, these are summarised below and compliance is demonstrated on the adjacent plan:

- The detailed access arrangement relating to plot 1-6 shall be a minimum of 4.1 metres wide for the first 10 metres;
- Sufficient turning space required for vehicles to enter and leave in a forward gear from Main Road access;
- The access off Main Road (for plots 1-6) shall show visibility splays of 2.4m x 65m in accordance with Manual for Streets for a 40mph carriageway;
- Plots 8 and 9 served off the existing private road (Spion Kop Lane) - there would be a requirement for the existing access to be improved for the first 10 metres. This surface should be made of a bound material to ensure that no loose material is strewn on the highway and a new kerb line, drainage and the construction of the vehicle access will be required to be in accordance with the highway authority's specification;
- A new footway would extend along Main Road around the corner onto Asperton Road and up to Spion Kop Lane;
- Two pedestrian crossing points would be included (one on Main Road and one on Asperton Road) to allow the safe movement of pedestrians.

Please note, the detailed design of the junctions and crossing points is ongoing.



2.8 PUBLIC AND PRIVATE SPACES

Policy 4: Design of New Development, of the Local Plan requires new development to clearly distinguish public and private spaces, the adjacent plan illustrates this.

The proposal includes the provision of a new footpath which would be available to all members of the public and create a safe pedestrian link along the northern edge of Main Road and the eastern edge of Asperton Road.

The footpath is bounded by a hedgerow which forms a definitive boundary as to the extent of the public area. Beyond this, a semi private area of soft landscape bleeds into the front gardens and hard landscaped areas for resident access and parking. A change in hard surfacing material provides a clear transition from the public areas into the development. Each dwelling has a private rear amenity space, enclosed by 1.8m boundaries..

LEGEND

Public area

Semi - Private area

Private area



2.9 PARKING PROVISION

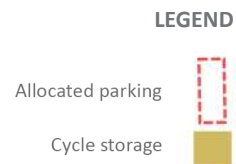
Parking Provision

To comply with Policy 31: Vehicle and Cycle Parking: of the Local Plan, the proposal has provided the required parking provisions as follows and illustrated on the adjacent plan:

- 2 spaces for dwellings of up to 3 bedrooms;
- 3 spaces for dwellings with 4 bedrooms;
- Where the garage has been allocated to parking, internal dimensions are in excess of 2.6m x 5.6m internal width.

Cycle Storage

Each dwelling has a dedicated cycle store, again highlighted on the adjacent plan.



2.10 WASTE MANAGEMENT PLAN

Waste Management

It is proposed to provide each plot with a dedicated bin and recycling storage space in the form of a flagged hard standing area, located within the private amenity spaces.

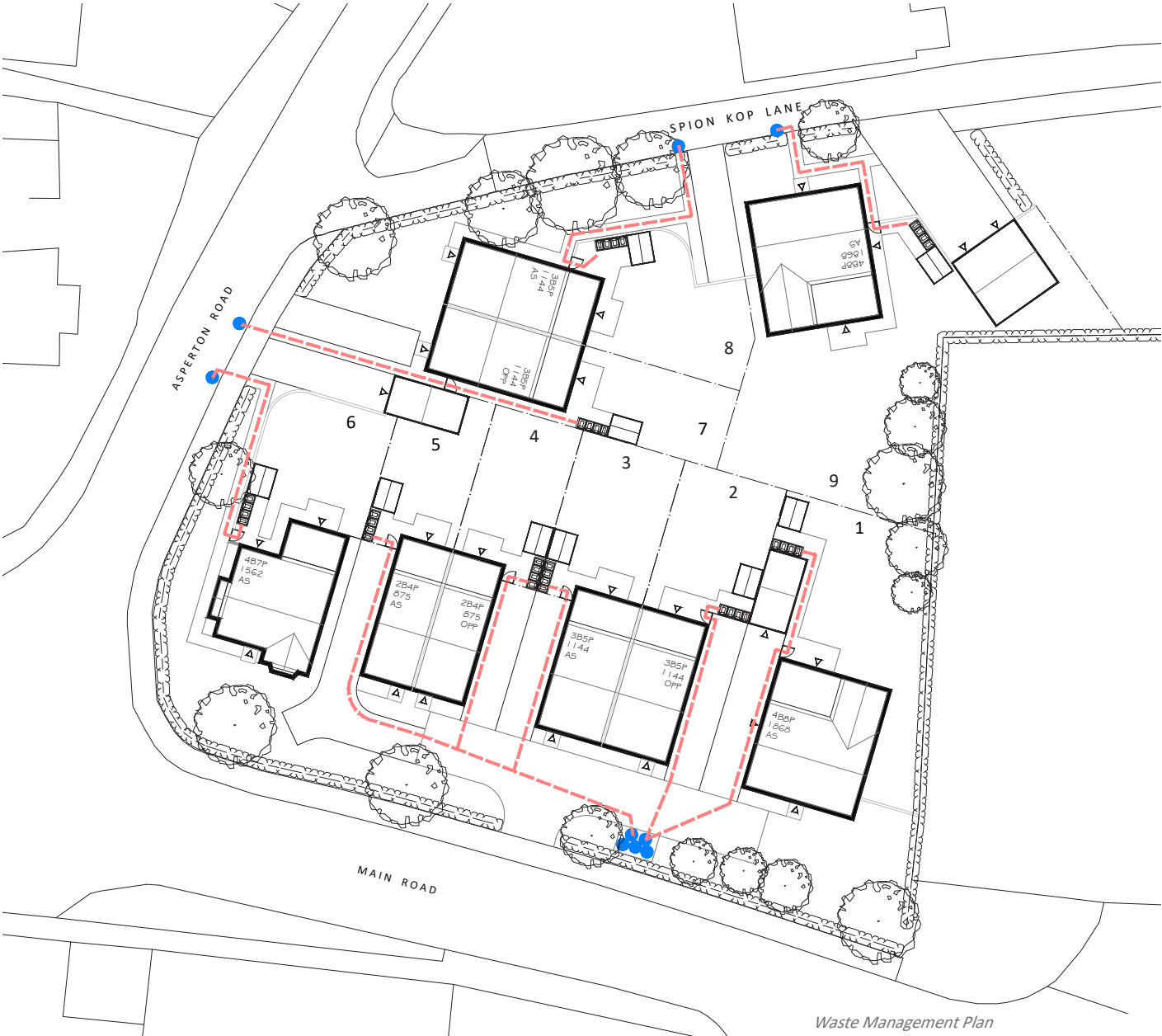
On waste collection days, occupants will place their bins in the locations demonstrated on the plan opposite.

Bin Collection point

Bin Storage Area

Bin Collection Route

LEGEND



Waste Management Plan

3. ARCHITECTURAL APPEARANCE



3.1 APPEARANCE

The vision for the site defined during the Outline application was:

'to create a well-designed development that will benefit the local area both socially and economically. This will be achieved by demonstrating how the design and layout of the development can draw from the strengths of the existing area and use these and other features to create a unique and distinctive development that will complement and enhance the character of the surrounding areas'.

Therefore, the dwelling façades have been designed to reference and enhance the character of the Conservation Area. Architectural details, prominent within the local vernacular, have been illustrated within the Heritage section. This chapter demonstrates how these architectural details have been incorporated. The design is intended to appropriately reference the vernacular with timeless detailing, without creating a pastiche development which would not age in harmony with the existing buildings.



STREET SCENE A-A



STREET SCENE B-B

Streetscenes



3.2 ARCHITECTURAL DETAILS



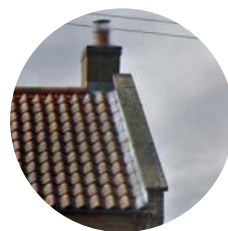
1. Orange pantile



2. Smooth orange brick



3. Multi tone brick



4. Parapeted gable



5. Chimney



6. Grey slate



7. Render

HT 03



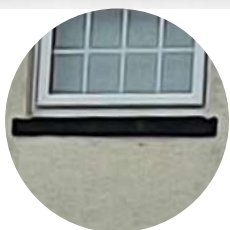
2no. HT 01



2no. HT 02



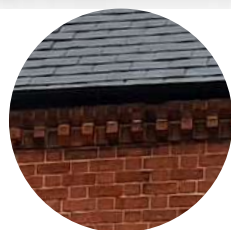
HT 04



8. Painted cills



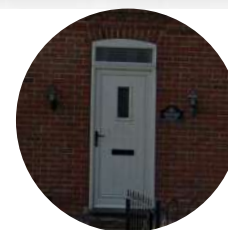
9. Splayed bay window



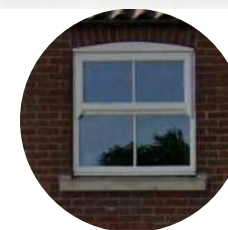
10. Dentil course



11. Stone fenestration detailing



12. Cottage style door



13. Modern alternative but traditionally styled windows

4. PLANNING CONDITIONS



4.1 PLANNING CONDITIONS

The OPP was granted with a number of planning conditions. Detailed design is ongoing with regards to some of the conditions however, this section sets out how the proposal has addressed a number of the conditions to date.

Conditions 1 states: *No development shall commence until details of the layout, scale, appearance, landscaping and access of the development (hereafter referred to as the 'reserved matters') have been submitted to and approved by the local planning authority.*

The Reserved Matters (RM) application which will comprise this document and other supporting information aim to illustrate the above.

Conditions 4 states: *The development hereby approved shall be carried out in accordance with the approved site location plan ref: 16013-002-01 Rev B, and no more than 9no. dwellings shall be provided on the site.*

This document and all drawings submitted as part of the RM application incorporate the same red edge as shown within the approved Location Plan (16013-002-01 Rev B).

Condition 5 states: *Notwithstanding the finished floor levels stated within the submitted Flood Risk Assessment (FRA) prepared by RM Associates (dated June 2017, Version 1) finished floor levels of the dwellings shall be set no lower than 300mm above existing ground levels. The remaining mitigation measures set out in the 'Flood Protection' chapter within the submitted FRA shall be fully implemented prior to occupation of each unit and subsequently remain in place.*

Detailed design of the dwellings is ongoing however, it is acknowledged the following measures will need to be incorporated (set out within the Flood Risk Assessment approved as part of the OPP) as well as the finished floor levels being set 300mm above existing ground levels:

The proposed residential units will have the following resilient measures incorporated in the construction in accordance with "Improving the flood performance of new buildings" CLG (2007).

- *The ground floor to be constructed with a solid concrete floor with no voids beneath and no low-level wall vents;*
- *Avoid the use of absorbent cavity insulation to the ground floor level;*
- *Treated and sealed timber skirting and architraves;*
- *Arrange for all service circuits to be routed at first floor level where practical socket outlets, boilers etc. to be a minimum of 0.5m above the raised ground floor level.*

Condition 12 states: *Prior to the commencement of the development above ground level, a Construction Management Plan shall be submitted to the Local Planning Authority for approval in writing. The Construction Management Plan will prescribe how the construction of the site will be phased, where site accommodation and welfare facilities will be placed, hours of working, where site vehicles and the vehicles of site personnel will be parked and where materials will be delivered and stored within the site. Construction of the permitted development shall be undertaken in accordance with the approved Construction Management Plan.*

A Construction Method Plan has been produced to accompany the RM application illustrating:

- The proposed phasing of construction;
- The location as to the site accommodation and welfare facilities;
- Proposed hours of working and access;
- Vehicular parking of site operatives and visitors;
- Loading and unloading of plant and materials;
- Details of construction traffic phasing
- Material delivery details;
- Details of storage of plant and materials for use within the development.

